

WESTERN PACIFIC

SUMMER 1981

Mileposts



526 Mission Street

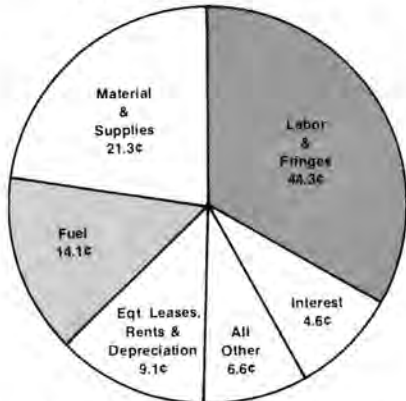
Many of you have noticed that the annual revenue of the Western Pacific Railroad has increased substantially in the last ten years. You may also have noticed that our profits have not increased accordingly. This is, of course, because of substantial increases in our costs. In fact, our railroad costs have increased faster than revenue has and we are now operating the railroad at a loss. In 1971 our operating revenues were \$84 million. By 1980 they had increased to \$185 million, a 120% increase. In the same period our total railroad

expenses had increased from \$81 million to \$188 million, a 132% increase. Fortunately, profits on the sale of property increased from \$1.3 million in 1971 to \$8.7 million in 1980 offsetting the railroad loss.

Inflation has been the major cause of these increased costs. The table below illustrates some of the increases we have experienced as compared to the Consumer Price Index. Our overall railroad cost increase of 132% is less than virtually all of these individual increases reflecting productivity and efficiency improvements at Western Pacific. Nevertheless, our costs have risen faster than the Consumer Price Index.

Cost Comparisons	Jan. 1, 1971	Dec. 31, 1980	% Inc.
Consumer Price Index	119.2	258.7	+ 117%
Typical Railroad Clerk			
Wage rate per hour	\$4.12	\$9.82	+ 138%
Fringe cost per hour	\$1.45	\$3.65	+ 152%
Rail per ton	\$172.95	\$470.59	+ 172%
Treated crosstie	8.19	20.36	+ 149%
Unequipped boxcar	\$19,000	\$44,000	+ 132%
Locomotive	\$230,000	\$650,000	+ 183%
Diesel fuel per gallon	9.48c	89.92c	+ 849%
Interest on equipment debt	7-3/4%	13-3/4%	+ 77%

Cost Components



The Pie Chart indicates the various components of our current railroad costs. Labor & Fringes are the largest single component of our costs at 44.3% per dollar. Material & Supplies are next at 21.3%. Fuel, our fastest growing component of cost was 14.1% of each dollar in 1980, up from 3.9% in 1971. The balance of our cost dollar is expended on equipment, interest and other miscellaneous expenses.

The inflationary cost increases we have experienced in the past will continue to be with us in the future. Since each and every Western Pacific employee can and does effect our costs, it is incumbent upon all of us to continue to limit or reduce expenses at each and every opportunity.



R. W. Stumbo, Jr.

R. W. Stumbo, Jr.,
Senior Vice President-Finance

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WESTERN PACIFIC'S 913

A report on the history of the famous F-7's history

CREW CONSIST

Successful Golf Tournaments and Picnics at Oroville and Graeagle/Portola

RULES & SAFETY

Successful Golf Tournaments and Picnics at Oroville and Graeagle/Portola

YARDMASTER TRAINING

Yet another craft receives special attention through training

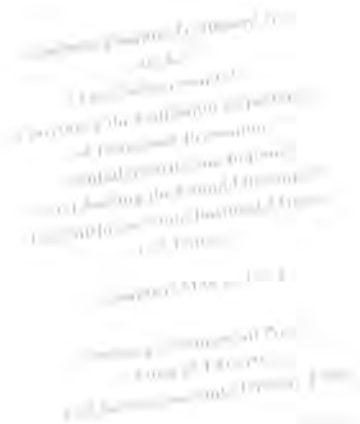
ON THE COVER



Western Pacific F-7 Diesel Engine 913 rests on the turn-table at the California State Rail Museum in Sacramento after presentation of the engine to the museum by WP President R. G. (Mike) Flannery. In front of 913 (L to R) are Chief Mechanical Officer R. W. Mustard; Rail Museum Curator of Artifacts W. Gray and Sr. V.P. - Operation R. C. Marquis.

Color Photos by R. E. Shideler
Stories pages 4 and 11

California State Railroad Museum GRAND OPENING



"A dream conceived 44 years ago is coming true with the grand opening of the California State Railroad Museum in Old Sacramento.

In 1937 the late Gilbert H. Kneiss, as founding chairman of the Pacific Coast Chapter of the Railway and Locomotive Historical Society, acquired the 'J. W. Bowker', an 1875 vintage steam locomotive, and the 'Silver State', an elegant narrow gauge passenger coach, built in 1881.

This was the start of the collection of historic engines and cars now on display in one of the world's finest railroad museums."

So begins the article written by Dick Rodda in the Official Program for Railfair 1981 held in Old Sacramento May 2 through May 10 in conjunction with the Grand Opening of The History Building at the California State Railroad Museum.

A Grand Opening it was -- that hot Saturday of May 2nd when, as the invitation states, the opening ceremonies began at noon. Over ten thousand dignitaries, railroaders, enthusiasts, their families and the just plain curious as well as Mileposts

gathered for this most significant and colorful event.

This was the first American Railfair in 32 years. The Railfair celebrated the opening of what is billed as the largest museum of its kind in the world, housing 22 pieces of historic railroad equipment, thousands of authentic artifacts of railroad history and folklore and over 40 interpretive exhibits. Rail Museum directors state, "The Museum provides interpretation of railroad history and its cultural impact on California and the West. More than 800,000 visitors are expected annually"

The new History Building completes the third and most ambitious phase of the Rail Museum project. The History Building joins the already completed Central Pacific Passenger Station and The Big Four Building which houses the Museum staff and is named after C. P. Huntington, Leland Stanford, Charles Crocker and Mark Hopkins -- the Sacramento merchants credited with building the first transcontinental railroad from the West.

The first nine days of the opening (May 2-May 10) were billed as the Railfair, featuring daily pageants and what seemed like a never ending stream of visiting rail equipment - some of which is shown on the pages immediately following this article.

The opening day crowd estimated at over 10,000 was almost as colorful as the equipment and other sights they came to view. Everywhere, engineer caps, kerchiefs and striped overalls were in evidence. Small children were perched on the shoulders of adults so they could get a better view. Enthusiasts were photographing pieces of equipment they had only read about. Some were measuring wheels and flanges with tape measures and calipers in hopes of constructing models at a future time - or to check on the authenticity of restoration. Grandfathers and fathers were explaining the fine points of railroading to the youngsters. In short, the opening of Railfair was a very important family happening.

When the speeches began at noon, more surprises were in store and particularly for Western Pacific. Stephen Drew, one of the organizers of Railfair, proudly announced that Western Pacific President R. G. (Mike) Flannery had offered to donate Western Pacific F-7 913, originally on loan to Railfair, as a permanent display in the History building. Of course, the Museum immediately accepted Mr. Flannery's offer. After the opening ceremony, Mr. Flannery told Mileposts that "after viewing 913 inside the Museum, I decided that it belonged there for all time."

On the pages that follow, Mileposts presents photographs of the Railfair with emphasis on some of the visiting equipment that has now departed. We hope you enjoy our mini-photo album which at least partially records the

significance of this event, particularly for those not fortunate enough to have attended.

A final note -- after completion of the Railfair, Mileposts returned to the Museum on June 2 to photograph the 913, the results of which you see on the cover and pages 12 and 13. Mr. R. C. Marquis, Senior Vice President-Operation and Mr. R. W. Mustard, Chief Mechanical Officer represented Western Pacific in an official presentation of WP 913 to the Museum represented by Mr. Warren Gray, Curator of Artifacts. Mileposts couldn't help but notice that a calming silence had replaced the din of the crowd on opening day a month earlier. Parked by the Central Pacific Passenger Station were four large yellow school buses from the Yuba City School District. Marching in neat rows were about 100 school children surrounded by their teachers, about to enter the History Building to see and enjoy part of their heritage. Maybe that's what the Museum really represents in the end. ☐

Visiting Equipment at Railfair . . .



Front view of Union Pacific's No. 8444 a fully operational 4-8-4 built in 1944.

VISITING EQUIPMENT AT RAILFAIR . . .

(Photos - Mileposts)



Long lines crowded to walk through Union Pacific's 3985 (Challenger), a 4-6-6-4 built in 1943 and restored to full operation in January of 1981. The Challenger is the largest operational steam locomotive in the world.



"Climax", a 3 truck design built in 1928 and sponsored by Western Washington Forest Industries Museum.



Atchison, Topeka & Santa Fe No. 5 "Little Butter Cup" an 0-4-0 circa. 1880.



A "Shaw" 3 truck n.e. built in 1884 and sponsored by the Michigan-California Lumber Co.



The Central Pacific Passenger Station.



Also a visitor to Ballair is Southern Pacific No. 1269, an 0-6-0 built in 1921.



A view inside the History Building. In the foreground is the Virginia & Truckee Railroad No. 12 "Genoa", a type 4-4-0 Baldwin built in 1873. To the right is Virginia & Truckee Railroad combination passenger car #16 built by the Detroit Car Co. in 1874.

... First, the shrill whistle, then the distant roar,
 The ascending cloud of steam, the gleaming brass,
 The mighty moving arm; and on amain
 The mass comes thundering, like an avalanche o'er,
 The quaking earth; a thousand faces pass---
 A moment, and are gone, like whirlwind sprites,
 Scarce seen; so much the roaring speed benights
 All sense and recognition for a while;
 A little space, a minute, and a mile.
 Then look again, how swiftly it journeys on;
 Away, away, along the horizon
 Like drifted cloud, to its determined place;
 Power, speed, and distance, melting into space . . .

(from an unidentified verse of over
 one hundred years ago)



Part of the crowd at the opening ceremony. Note R. G. (Mike) Flannery at left center. This photo, taken immediately after the ceremony, catches Mr. Flannery smiling after announcing the donation of WP 913 to the Railroad Museum.



The "913", first a visitor and now occupying a permanent place of honor inside the History Building.

Western Pacific's 913

By Ken Meeker

Stockton, California is the home of Western Pacific's main locomotive facility and also the site of the largest freight yard on the railroad. Stockton is also home base for what are now the oldest mainline diesel locomotives operating in the state of California, specifically known by most railroaders as "covered wagons". In the beginning of 1981, WP still owned four covered wagons, the nickname for builder Electro-Motive Division's model designation F-7. WP at one time owned over 100 "F-units" of both freight and passenger design, the most famous of which were the silver and orange passenger units that pulled the **California Zephyr**, the WP's crack streamlined passenger train that ceased operation on March 20, 1970.

Over the years, new innovations in diesel locomotive design led to the replacement of the covered wagons on the WP and other railroads throughout the country, and now only a handful are left. The covered wagons gained popularity among train crews as their solid construction and excellent visibility made operation much safer than steam, and they could pull heavy trains

at slow speeds remarkably well. On a railroad like WP with mountainous grades in the Feather River Canyon and on the Highline, this was a big advantage. The covered wagons were also responsible for the eventual demise of the steam locomotive during the 1950's and for the next twenty years they ruled the rails on the WP. As 1970 rolled around however, only four covered wagons remained on the active WP roster, a fact that made them very special among employees and railroad enthusiasts alike. For many of the younger employees they were the last link to the Zephyr Era and for some oldtimers, they represented an entire career. Rail enthusiasts paid particular attention to the four units because they were the last of a dying breed and the only ones operating in California.

All four of the remaining covered wagons, numbers 913, 917, 918, and 921 were built in 1950 and "renumbered" in 1975 from 913-A, 917-D, 918-D, and 921-D, respectively. In addition, the 913 had swapped numbers with the 920-A in 1971.

The 913 and her three sisters gained popularity and fame in the early 1970's while in service out of Stockton where the aging locomotives were assigned for maintenance purposes. Twice a day, seven days per week, the quartet would operate between Stockton and Milpitas/San Jose on the afternoon and night San Jose trains, logging over



F7A #913 and her three sisters pose for photographers at Stockton during July, 1978. Photo by Ken Meeker, San Carlos, CA.

The 913

THE WESTERN PACIFIC'S 913
Built 1950
Mfg. Electro-Motive Division of
General Motors
Weight 244,700
Horse Power 1500
Max. Speed 65 Miles Per Hour
Equipped with Snowplow
and Flangers
Designed to Drag 3800 Ton at
11.5 Miles Per Hour on Level
Track.
Donated to California State
Railroad Museum and retired
from active service May 2, 1981.



300 miles each day while hauling important tonnage over Altamont Pass and the Coast Hills. Occasionally during the winter months, the 913, which was equipped with a pilot snowplow, would be sent east to Keddie for plow train service in the Feather River Canyon and on the Highline (NCE). The cowl-shaped nose and enclosed carbody of the 900-class units make them ideal for fighting the deep snow drifts since snow cannot accumulate on the nose as is the problem with the newer hood units that have square noses. When the snow seasons were over, the 913 would be returned to Stockton for continued service in the San Jose pool.

During the late 1970's, the vintage locomotives began to show signs of wear and tear as the years of hard service began to take toll on their everyday performance. There was also the possibility of an order for new motive power that would have spelled the end for the aging quartet. In 1977, however, thoughts of new motive



WP F7 #913 in snowplow service at Almanor on Highline during January, 1977.

Photo by Ken Mosker, San Carlos, CA

power were temporarily postponed and instead the decision was made to rebuild the four covered wagons with hopes of receiving at least three more years of service from the veteran locomotives. This decision gained much attention among interested employees and railroad enthusiasts alike as it would mean a new lease on life for the F's and new paint jobs, something that the battle-scarred locomotives desperately needed.

All four covered wagons were taken out of service at one time or another during 1977, and while the necessary repair work was being performed, numerous WP employees and friends of the railroad came up with a suggestion that would perhaps bring back a little of the California Zephyr tradition that gained so much public attention and employee pride during the Zephyr Era. It was suggested that when it came time to paint the covered wagons, that one of them be adorned in the silver and orange colors of the Zephyr Era. This proposal was drafted into a letter to WP President "Mike" Flannery who liked the idea and gave permission for Stockton's Shop Superintendent "Scotty" Gault to order the special paint. At the same time a search began for a square "Feather River Route" herald that would be attached to the nose of the silver and orange diesel as was the practice during the Zephyr years. These attractive emblems were long since out of production since the arrival of the "new image" green paint in 1970 when locomotive numbers or a block "WP" replaced the herald. Fortunately, WP Locomotive Engineer Norman Holmes of Portola, California was willing to donate an emblem from his railroadiana collection and everything was then set. Next came the easy part — deciding what covered wagon to dress in the special paint. All those involved in the project had the same choice—the 913. The reasons were simple; one, the 913 lacked multiple-unit connections on its nose, therefore it would have to operate as either the lead or trailing unit in every consist and, two, 913 was the number of the WP's first F-7.

On March 7, 1978, the 913 was switched into the paint shop at Stockton's roundhouse and painters Art Alvillar and Henry Casarez went to work sanding down the 28-year-old carbody in preparation for its historic paint job. Six weeks later, on April 21, 1978 the two painters put the finishing touches on their pride and joy and the 913 was released for revenue service. In reality the shiny covered wagon was adorned in a unique paint scheme, combining both the freight and passenger schemes of the Zephyr Era.



Teamwork: painters Art Alvillar and Henry Casarez apply orange paint to flanks of 913 at the Stockton paint shop. March, 1978. Photo by: Ted Benson, Modesto, CA.



Painters Art Alvillar and Henry Casarez pause for an informal portrait with completed 913 on a cloudy April, 1978 afternoon in Stockton. Photo by: Ted Benson, Modesto, CA.

On April 24, 1978, F-7's 913, 917, 918 and 921 were assigned to the afternoon's San Jose Turn, the first time that the four covered wagons had operated together in over nine months. With the 913 on the point, of course, and her three sisters all now adorned in new green paint with orange trim, the four locomotives made a colorful consist.

For the long-awaited "first run", the silver and orange diesel wore the original "913-A" numberboards and flew white flags to commemorate the occasion. The crew for the first run consisted of all veteran railroaders, Engineer Ed Hale, Conductor Ralph "Bowser" Burke and Brakemen Ermon Bills and Bob Morris. Together the four



WP F7 #913's "First Run" in new colors on the San Jose Turn at Hearst, CA on 4-24-78. Photo by Ken Mosker, San Carlos, CA

men had a total of over 100 years of railroading experience under their belts. It was only fitting that Ed Hale should be the hoghead for this historic trip as the 41-year veteran previously had made the transition from steam to diesel thirty years ago, by breaking-in the WP's first covered wagons on the rugged Fourth Subdivision or "High-line" between Keddie and Bieber. According to Ed, "These 900's are still the best darn mountain engines we've got!" Ed and Bowser were the **Turn's** regular engineer and Conductor, respectively, and both were due to retire within the next couple of weeks, making the occasion even more meaningful.



Ed Hale waits for the highball on the San Jose Turn, getting an air test before leaving Stockton in May, 1978.

Photo by: Ted Benson, Modesto, CA.

The first run of the 913 in its new colors was well recorded by photographers, as employees and enthusiasts alike lined the railroad's First Subdivision to witness the event. Even a rainy and overcast day couldn't dampen the enthusiasm and the occasion gained much coverage in railroad-oriented magazines.

The 913 was soon to become a showpiece and eventually the talk of the railroad as employees were pleased to see that there was still room for a little history and tradition on a modern railroad like WP. Nationwide coverage of the WP project soon saw other large railroads throughout the country repaint one or two of their locomotives into now-obsolete paint schemes; as the WP it seemed had started a trend.

For the next three years, WP's covered wagon quartet continued to pound the rails between Stockton and San Jose or Oakland, hauling tonnage to and from the Bay Area. The four locomotives had however, logged many miles over the years and the daily grind finally began to take its toll. The three years life expectancy granted to the F's during their rebuilding in 1977 soon became reality and as the winter of 1980 rolled around, mechanical ailments often sidelined them for long periods of time. Newer hood locomotives and leased power from the Union Pacific began to infiltrate the San Jose trains. The 913, by this time, had become very famous and WP was invited by the State of California to display the silver and orange F-unit during Railfair Week from May 2 through May 10, an event that would commemorate the opening of the State's huge railroad museum at Old Sacramento. All of California's large railroads were sending display locomotives and the 913 was the logical choice to represent the WP.



913 and two green sister covered wagons flash past the old Carbona (Tracy) depot with the Santa Fe Autos eastbound SJT return from Milpitas. July, 1978.

Photo by: Ted Benson, Modesto, CA

The morning of February 18, 1981 found the 913 and her three green sisters in charge of symbol **SJM** at Milpitas, California, ready to head east with Bay Area perishable and canned goods traffic. As Engineer Jon Droivold prepared his power for the pre-sunrise journey to Stockton, little did he know that it would be the last trip for the 913 in revenue service on the WP. Upon arrival back at Stockton the silver and orange

covered wagon was taken out of service and put back into the paint shop to receive a fresh coat of the special paint in preparation for its display at the museum. At the time no one knew that it would never return.

Painters Avillar and Casarez again recreated their masterpiece and on



Restored to Zephyr silver and orange once again, 913 is displayed inside the California Railroad Museum at Sacramento in May, 1981.

Photo by: Ted Benson, Modesto, CA.

April 23, 1981, ironically one day before the three-year anniversary of the 913's historic "first run", the freshly-painted locomotive was shipped dead-in-train to Sacramento for eventual delivery to the state's museum, a place the 31-year old locomotive would now call home.



The crew who prepared the 913.

Left to right: Painter - Art Alvillar; Carpenter - Frank Driscoll; Foreman - Phil Burch; Painter - Henry Casarez; Boilermaker - Richard Radcliff

Crew Consist Arrives on WP

Negotiations which began in 1964 finally culminated in Western Pacific's Sixth Floor Board Room on May 13, 1981 when Company officers and the representative of the United Transportation Union signed a Crew Consist Agreement. Participating in the signing ceremony were General Chairman H. A. Siler for the United Transportation Union and R. C. Marquis, Senior Vice President-Operation; T. R. Green, Director-Personnel and M. A. Hartman, Manager-Labor Relations, for the Company.

Since 1964 the issue of train crew size has been the subject of intensive and lengthy bargaining, both at the national level and on the separate properties. The Agreement signed May 13th had its genesis in a Company proposal made in June of 1977 after the national bargaining committees had decided that the issue was one more properly handled on the separate properties, rather than on a national basis.

For many years a minimum train crew of one conductor and two brakemen has been required by the collective bargaining agreement. The new Crew Consist Agreement amends that requirement by permitting certain road and yard assignments to be manned by one conductor and only one brakeman.



Left is UTU General Chairman H. A. Siler shaking hands after signing the Agreement with M. A. Hartman, Director of Labor Relations for the Company.

The Crew Consist Agreement represents at once both a recognition by the Union and the Company that there has been over the past several decades a gradual but serious erosion of railroad traffic to other modes, chiefly to trucks, and an attempt by the parties to turn that situation around. It is the hope and purpose of the parties to the Crew Consist Agreement that the savings and efficiencies eventually to be realized from that Agreement together with the rate-making flexibility recently granted the railroads by federal legislation will make available the funds necessary to meet other modes of competition and recover a substantial portion of lost traffic and also to entice new business to the rails. If these hopes are realized, the result will be an increase in productivity which will provide more jobs with enhanced security and a prosperous, financially-stable railroad.

The Agreement provides that it will become effective upon thirty days' advance written notice served by the Company. Since the date of execution of the Agreement, the Company has

been conducting orientation meetings for the personnel who will be involved in the day-to-day application of its terms in order to insure to the greatest extent possible their uniform administration. Service of the implementing notice will be made some time after the orientation program has been completed. ☑



(L to R) Sr. VP-Operation R. C. Marquis, Director Personnel T. R. Green and UTU General Chairman H. A. Siler.



The Enemy

I am more powerful than the combined armies of the world. I have destroyed more men than all the wars of all nations. I massacre thousands of people every year. I am more deadly than bullets, and I have wrecked more homes than a cabal of the world's most sensuous women.

In the United States alone, I steal over \$500 million each year. I spare no one, and I find victims among the rich and poor alike, the young and old, the strong and weak. Widows know me to their everlasting sorrow. I loom up in such proportions that I cast my shadow over every field of labor.

I lurk in unseen places and do most of my work silently. You are warned against me, yet, you heed me not. I am relentless, merciless and cruel. I am everywhere — in the home, on the streets, in the factory, at railroad crossings, on land, in the air and on the sea.

I bring sickness, degradation and death, yet few seek me out to destroy me. I crush, I maim, I will give you nothing and I may rob you of everything you have.

I am your worst enemy — I am Carelessness.



Electrical Foreman Richard Lightle (left) presents a Western Pacific safety jacket to electrician Gene Marler respecting the fact that Gene had not had a reportable injury in the last fourteen years. Turn about being fair play, Richard was also presented a safety jacket recognizing eighteen years without a reportable injury.

Fifth Series of Golf Tournaments and Picnics In Oroville and Portola . . . A Huge Success

Photos: Oroville - Paul Gordenev

Graeagle/Portola - Lisa Kroeber

As previously announced in the Spring issue of Mileposts, the first two of our four annual golf tournaments and picnics took place during this quarter. Over five hundred employees and their families and friends attended each of the outings in Oroville on May 9th and at Graeagle/Portola on June 20th. Both were an undeniable success as anyone who attended will attest.

A great round of applause is due our Operating Department which

organizes and conducts these affairs and most particularly to Andy Kinicki, Director-Rules and Safety, H. E. Baldwin, Al Hill, Dorothy Smith, Don Dali, Bill Wheeler, George Barnes, Bob Brew, Karen Thomas, Jack Rich, Leonard Moser and many others too numerous to mention.

Several events besides the golf tournaments were held at each location. Mileposts congratulates the winners and here prints the results.

Oroville - May 9th*

Golf Tournament Table Mountain Golf Course

WOMEN

Low Gross	Low Net
1st - T. Gomez	1st - M. Hill
2nd - B. Luebke	2nd - E. Conant
3rd - M. Rumsey	3rd - M. Metzdorf
Longest Drive - F. Elliott	
Closest to the hole - T. Gomez	

MEN

Low Gross	Low Net
1st - J. Warren	1st - M. Driscoll
2nd - J. Rogers	2nd - G. Newhart
3rd - L. Gomez	3rd - T. Cutter
Longest Drive - J. Warren	
Closest to the hole - H. Dopp	

Graeagle/Portola - June 20th

Golf Tournament Graeagle Meadows Golf Course

WOMEN

Low Gross	Low Net
1st - M. Green	1st - P. Furtney
2nd - B. Treanor	2nd - E. Hastings
3rd - L. Quill	3rd - F. Marquis
Longest Drive - L. Quill	
Closest to the hole - S. Fisher	

MEN

Low Gross	Low Net
1st - G. M. Hugg	1st - R. Christensen
2nd - J.H. Rogers	2nd - G. Metzdorf
3rd - S. Thomas	3rd - F. Reith
Longest Drive - T. Terhorst	
Closest to the hole - R. A. Issac	

*Note: 100 golfers entered this tournament. A total of 10,140 strokes were recorded. Handicap strokes were 7,208.

Races North Forebay Recreation Area

Sack Races

3 Legged Races

Age 5 and under

Juan Amaya Kera Metzdorf	1st Place 2nd Place	Juan Amaya & Kera Metzdorf
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Age 6 through 8

April Mayle Tamatha Mayle	1st Place 2nd Place Tie 2nd Place	April & Tamatha Mayle Gina & Donny Wheeler Jody Gardner & Michael Sawyer
Angea Downer	3rd Place	Adreanne Metzdorf & Kendyle Bird

Age 9 through 12

Vicki Edwards Kathy Mayle Theresa Mayle	1st Place 2nd Place 3rd Place	Vicki & Cheryl Edwards Katherine & Theresa Mayle Stephanie Witmore & Lynn Gardner
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Age 13 through 15

No entries

Age 16 and over

Connie Couch	1st Place	Connie & Denise Couch
	Tie 1st Place	Becky Bergstrand & Rich Hard
Becky Bergstrand	2nd Place	D. P. Mayle & A. G. Amaya
	3rd Place	Sandy Fischer & Ron Couch

Horseshoe Tournament Portola City Park

WOMENS DOUBLES

Champion - Pat Scoville, Mae Dillard
1st Place - Jeanette Wilkinson, Grace Robinson
2nd - Place - Kim Schake, Carrie Schake
3rd Place - Marie Lawrence, Ruth Phelps

MENS DOUBLES

Champion - Jeff Reith, Douglas Weaver
1st Place - Andy Kinicki, Lee Lawrence
2nd Place - Laurel Fisher, Walter Fisher
3rd Place - Glen Metzdorf, Jr., Layne Shields

The Union Pacific Railroad German Band



Members include: Accordion - Bill Erickson; Clarinet - Joe Genovesi; Trombone - Ivan Summer; Tuba - Vern Luddington; Trumpets - Frank Siedlik and Bob Jeperson; Drums - Brian Casterline (unfortunately not pictured here).

The Union Pacific Railroad very kindly sent their seven piece GEMULICHKEIT German Band to the Portola picnic for our enjoyment. Head quartered in Omaha, Nebraska, the band found itself a long way from home but we hope they found Western Pacific hospitality as much to their liking as we did their music.

The following pages present a portfolio of various scenes at the two outings. Remember that two more golf tournaments and picnics are scheduled this year - Elko on August 22nd and Pleasanton on October 17th. See you there! ☑

OROVILLE . . .



PORTOLA . . .





Railroad Retirement Board Release:

Statements of Service Issued by Railroad Retirement Board

The U.S. Railroad Retirement Board recently completed the preparation of the Certificates of Service Months and Compensation, Form BA-6. These annual statements are important because they provide an employee with a record of his or her service months and creditable compensation in 1980, as well as cumulative railroad retirement credits after 1936. The statements are also used by employees when they apply for unemployment or sickness benefits with the Board.

This year, BA-6 forms have been prepared for all employees who worked in 1980, regardless of the number of months worked or the amount of compensation earned. Most employees will receive them directly from the Board by mail in mid-June, although some BA-6 forms will be distributed through employers. Employees who worked for a railroad in 1980 and do not receive a Form BA-6 by June 30, or need a replacement form due to loss, may obtain one by visiting any Board office.

Each employee should check to see whether his or her own record of service and creditable compensation agrees with the Board's figures shown on the BA-6 form. The important figures that should be checked are indicated on the illustration of the form. In checking the 1980 compensation total, employees should be aware that only earnings up to \$2,158.33 per month were creditable for railroad retirement in that year. Therefore, the maximum amount of creditable compensation that can be shown on the form for 1980 is \$25,900. If there is a name difference, employees should keep in mind that the form contains only the first ten letters of the last name. In addition, employees who first worked for a railroad in 1980 may receive BA-6 forms showing only the first 5 letters of their last name, followed by an asterisk. This is not a name discrepancy.

Any employee who receives a BA-6 form with an incorrect name or social security number can obtain a corrected form if he or she brings proper identification to any Board office. Any other

What Employees Should Check on Form BA-6 . . .

What Employees Should Check on Form BA-6

1 Name

2 Social Security Number

3 Total Service and Compensation Under Railroad Retirement Act, 1937-1980

4 Service and Compensation Credited Under Railroad Retirement Act in 1980

discrepancies in BA-6 forms should be reported to the DIRECTOR OF DATA PROCESSING AND ACCOUNTS, U.S. RAILROAD RETIREMENT BOARD, 844 NORTH RUSH STREET, CHICAGO, ILLINOIS 60611. The employee should include his or her social security account number in the letter; otherwise, no corrective action can be taken by the Board. The time limits for correcting errors are shown on the back of the BA-6 form, along with certain Medicare and supplemental annuity closing date reminders.

New Benefit Year for Railroad Unemployment and Sickness Benefits

July 1 is the start of the 1981-82 benefit year under the Railroad Unemployment Insurance Act. This Act, which is administered by the U.S. Railroad Retirement Board, provides 2 kinds of cash benefits for qualified railroaders: (1) unemployment benefits for those who become unemployed but are able to work and available for work, and (2) sickness benefits for those who are unable to work because of sickness or injury.

The following questions and answers describe railroad unemployment and sickness benefits and explain how to apply for these benefits.

1. What are the service and earnings requirements for railroad unemployment and sickness benefits in the 1981-82 benefit year?

Normal benefits starting July 1981 are based on an employee's railroad service and earnings in calendar year 1980. To qualify for normal railroad unemployment or sickness benefits, an employee must have had railroad earnings of at least \$1,000 in calendar year 1980, not counting more than \$400 for any month. Those who were not employed in the rail industry before 1980 must also have worked in at least 5 months of that year (not necessarily full-time in each month).

Under certain conditions, railroaders who do not qualify on the basis of their 1980 earnings may still be able to qualify for benefits in the 1981-82 benefit year.

2. I did not work in 1980. Under what conditions could I receive unemployment or sickness benefits in the benefit year beginning July 1, 1981?

If you received normal benefits in the July 1980-June 1981 benefit year, you

As shown in the illustration, the BA-6 form has detachable sections at each end. The section on the right side is used when the employee applies for sickness benefits, and the one on the left is used for unemployment benefits. The reverse sides of these sections also explain the best way to apply for these benefits. The use of these sections prevents delays in the payment of unemployment or sickness benefits which occur when an employee enters the wrong social security account number on an application for benefits.

may be eligible for extended benefits; or, if you have 10 or more years of service and would be qualified in July 1982 based on your 1981 earnings, you may start the next benefit year early. For example, if you earned \$1,000 or more in January-June 1981, counting earnings only up to \$400 a month, you could be eligible to receive benefits in the benefit year beginning July 1, 1981. However, sickness benefits paid on this basis are terminated at age 65.

If you think that you may be eligible for benefits, you should file an application; the Board will notify you regarding your eligibility.

3. How much can unemployment or sickness benefits amount to?

Almost all employees qualify for the maximum daily benefit rate of \$25, which yields \$250 for two full weeks of unemployment or sickness. The daily benefit rate for unemployment or sickness benefits is 60% of an employee's last daily rate of pay in the qualifying base year, but no more than the maximum benefit rate of \$25.

4. How long are unemployment or sickness benefits payable?

Normal unemployment or sickness benefits are each payable for up to 26 weeks in a benefit year. The total amount of each kind of benefit which may be paid cannot exceed the employee's base-year earnings, counting up to \$775 per month.

If an employee with 10 or more years of service exhausts normal benefits, he or she may qualify for extended unemployment or sickness benefits. For employees with 10-14 years, an extended benefit period could last for up to 13 consecutive weeks. Employees

with 15 or more years of service may qualify for up to 26 consecutive weeks of extended benefits.

During certain periods of high unemployment, employees with less than 10 years of service may qualify for up to 13 consecutive weeks of extended unemployment benefits. On July 1, 1981, such a period of high unemployment had been in effect since mid-1980.

5. How should I apply for unemployment benefits?

Visit a railroad unemployment claims agent during the first week you are unemployed; this will prevent any loss of benefits. There is probably a claims agent where you were employed, or a supervisor can direct you to one. If possible, bring your most recent Form BA-6, "Certificate of Service Months and Compensation," because this form helps the Board to expedite payments.

The claims agent will assist you in applying for benefits and will advise you when to return to reapply if you remain unemployed.

6. How do I go about applying for sickness benefits?

An application for sickness benefits (Forms SI-1a&b) can be obtained from a railroad employer, a railroad labor organization, or any Board office. The Board suggests that you keep a sickness benefit application form on hand, and show your family members where it is kept and how to use it on your behalf. Also, it is a good idea to keep your latest Form BA-6 with the application. Form BA-6 shows your social security number and includes instructions on how to claim benefits.

Within 7 days after the first day for which you want to claim sickness benefits, complete the application, have your doctor complete the medical statement, and mail both sections of the application to the Board. Payments will be expedited if you enclose the right-hand portion of your most recent Form BA-6, but don't delay sending your application if you are unable to locate your Form BA-6.

7. I mailed my sickness benefit application late, because my doctor didn't complete the medical statement on time. Will I lose sickness benefits because of late filing?

If late filing was solely due to your doctor's delay, and if you notified the Board accordingly, you would not lose any sickness benefits.

If an application is filed late, some or all of the sickness benefits must be denied unless a good reason for the delay is furnished to the Board. If you cannot obtain a sickness benefit application on time, or if hospitalization or a doctor's delay prevents timely filing, or if any other circumstances beyond your control prevent timely filing, be sure to enclose an explanation with the sickness benefit application.

8. Are railroad unemployment and sickness benefits taxable?

Unemployment benefits may be taxable under Federal income tax laws. In addition, sickness benefits paid for days on which the beneficiary would otherwise have been receiving unemployment benefits may be subject to Federal income taxes. Taxability of these benefits depends on the total income and filing status of the beneficiary.

For more information, obtain a copy of Internal Revenue Service Publication 905, "Income Tax Information on Unemployment Compensation," from the Internal Revenue Service or any Railroad Retirement Board district office.

Railroad Retirement Annuity Increase

Railroad retirement annuitants, as well as social security beneficiaries, will receive cost-of-living increases in their monthly benefits, beginning with their July 1 checks.

The tier I portions of all employee and spouse railroad retirement annuities, and both the tier I and tier II portions of survivor annuities, will be increased by 11.2%. These increases, under automatic cost-of-living provisions, are the same as for social security benefits. However, if an annuitant also receives a social security benefit, the social security cost-of-living increase is subtracted from the railroad retirement tier I increase.

Current law also provides a separate 3.6% increase in certain tier II components of employee annuities, and in the entire tier II portion of spouse annuities. These increases are not reduced because of the beneficiary's entitlement to social security benefits.

Dual benefit windfall components, payable over and above tiers I and II to preserve pre-1975 dual benefit rights, are not subject to these increases. Nor are any cost-of-living increases applied to supplemental annuities paid by the Railroad Retirement Board.

Increases not paid on July 1 will be paid

retroactively at the earliest possible date thereafter.

Also, the monthly Medicare premium will be increased for both railroad retirement and social security beneficiaries. The basic Medicare premium will increase July 1 by \$1.40, raising the premium from \$9.60 to \$11.00.

Yardmaster Training Conducted In Oakland

During May and June, a total of forty Western Pacific and Sacramento Northern Yardmasters and Approach Yardmasters attended a four-day training program at the Oakland Training Facility. This first phase of Yardmaster Training centered on supervision, communication, the Field Transportation System, the Terminal Information System and other system-wide policies and procedures necessary for a common understanding of all yardmasters.

Each yardmaster who attended will make trips on the property in accordance with the needs of his home terminal. All yardmasters will spend at least part of a day in the dispatching office located in Sacramento.

A second phase of this training program will encompass specific terminal policies and procedures and will be conducted by the senior transportation officer at each terminal beginning in early July. ☑

The program is coordinated by the Personnel Department in a joint venture with the Operating Department.



Trainmaster F. D. Webb, one of several instructors makes a point during a discussion in one of the yardmaster training sessions.



OAKLAND/SAN FRANCISCO YARDS Rose Ganassin & Flo DeSouza

Over in Oakland, the city of "Billy Ball", the Western Pacific Railroad is making its contribution to all the sports fervor by sponsoring a slow-pitch softball team with the Oakland Industrial Recreation Association. Playing in the Co-Rec League (1/2 men - 1/2 women) are the WP "Deadheads"; a team made up of engineers, switchmen, clerks and their families. The team's games will be on Monday nights throughout June & July. Those wishing to attend games or obtain a team jersey, can contact the Oakland Yard office.

As B. P. Lewis, manager of the team says, "With a name like the 'Deadheads', we need all the support we can get."



Photograph Jim Swan, Claim Agent, Oakland

The above pictured WP engine now adorning Florbele de Souza's desk, Oakland, was made by her son, Ivan de Souza, age 9 years, as part of a Cub Scout project known as the "Genius Kit". As you can see the engine was made from roll of cardboard, wire clothes hanger, block of wood, popsicle sticks, etc. When you are visiting Oakland Yard, be sure to take the time to stop by Flo's desk and view this creation. It's adorable!

SAN FRANCISCO GENERAL OFFICE

E. G. "Sam" Razo was honored at a luncheon at the Seven Seas Restaurant on May 27th in honor of his retirement from active service May 31st. Sam, who retires with more than 28 years service, was Chief Investigator in the Freight Claim Department, in which he had worked since 1964. In his career, Sam had also served in both the Engineering and Personnel Departments. Over 50 attendees looked on as Sam was presented a certificate attesting his service and a 'gold spike' by BRAC and WP. Sam will be missed by his fellow employees as well as many of



*(Top Row l to r) B. P. Lewis, D. S. Partridge, R. Pakiser, L. Morningstar, E. Kniemeyer, R. L. Cardenas, J. Perales, K. Meeker
(Bottom Row l to r) R. B. Millar, L. McGeary, R. Partridge, C. Pirie, L. Wheeler, A. G. Ray*

Switchman, Mark Villa just returned from Dayton, Ohio after playing in the national rugby competition. He is a member of the "Old Blues" Rugby Club from Berkeley, California, which consists of University of California alumni. They won the national title three times and boast an impressive record of 65 wins; 5 losses and 2 tied games!

our customers with whom he had built such fine rapport over the years. Good luck in a well-deserved retirement, Sam.



Director Quality Control & Freight Claims Leo Battaglia presents certificate honoring more than 28 years service to retiree E. G. "Sam" Razo. Watching the presentation are, to Sam's left, his wife, Isabel and Dora Prophet. Seated to Mr. Battaglia's right is Anne Irwin.

FREMONT Norma Lill

Pictured below are Engineer Charles Otis and his wife Marie at a Cake and Coffee send-off celebrating Charles' retirement on April 30th with more than 37 years service.



Marie and Charles Otis

Also departing Fremont to move to Portola on June 25th was Brakeman Bill Thompson. Fremont's loss is Portola's gain!

This correspondent's daughter, Lt. Rhonda Weldon, was married to Jay Gibson of Chico, California at the Mare Island Chapel on May 9th. The newlyweds will reside in Waukegan, Illinois where Rhonda is a Navy Nurse at the Great Lakes Naval Hospital.

STOCKTON YARD Elaine Obenshain

On June 5, 1981, Michael McCullough graduated from the Franciscan School of Theology of Graduate Theological Union in Berkeley, California, having completed his training for the priesthood. Proud sister is Clerk Mary McCullough, Stockton Yard Office; proud dad is retired Clerk Bill McCullough.

Retired Carman Cecil Smith visited the office in May, looking fit and trim, and enjoying his retirement to the fullest.

Bkman and Mrs. David Kirkpatrick are proud parents of Kristopher David, born May 14, 1981, weight 9 lb. 9½ oz. and 19-3/4 in. tall. Pleased grandparents are Condr. and Mrs. Glenn Kirkpatrick - bragging about their first grandson after having six granddaughters. Great-grandfather is retired Roundhouse Foreman Charles B. Kirkpatrick.

Retiring May 29th were Condr. Gerald T. Clark, who entered service May 16, 1937, and Engr. John A. Osterdock, who entered service June 9, 1937. Gerry was presented with a scale model of a caboose and John with the scale model of an engine. We wish them many happy years of retirement.

Carman Wayne Orton received his 35-year service pin May 1, 1981 and retired May 29, 1981. He was honored with a cake and visited by several retirees.

S.N. Carman Joe Hahn (Pittsburg) attained 35 years service in May 1981, requesting the necklace for his wife. In addition was given the tape measure with the W.P. emblem which is a safety award.

STOCKTON CAR DEPARTMENT AND SHOP

Ralph Patton and James Mendoza
Graduation time!

Terri Phillips, daughter of Carman-Write-Up Arnold Phillips, graduated from Linden High School.

Kim Hurley, daughter of Carman George Hurley, graduated from Edison High School.

Angelo Martin Mendoza, son of Carman-Write-Up James "Spike" Mendoza, graduated from Franklin Senior High.

Angela Jean Patton, daughter of Car Department Clerk Ralph Patton, graduated from St. Luke's Grammar School 8th grade and will be attending St. Mary's High School in the fall.

SACRAMENTO SHOPS Jean Smith

The Western Pacific Amusement Club, Sacramento Shops, held its "Old Timers" Night on May 4, 1981. The following retired employees attended: H. J. Madison, C. C. Bennett, D. D. Davies, A. E. Drummond, A. Stadler, J. D. Flippin (93 years old), M. Velasich, J. Teaverbaugh, F. Goncalves, C. P. Rolfe and A. P. Springer. H. J. Madison donated a bottle of spirits to be raffled off to the "Old Timers". The winner was A. E. Drummond. The winners of three other drawings were F. Goncalves, H. J. Madison and C. P. Rolfe.

Best wishes for a speedy recovery to Guy L. Langton, Machinist, who has been off work due to surgery since April.

Congratulations also to Carman Don Swaringen who became a grandpa on May 19 for the first time. He is mighty proud of his little grandson.

Donuts and coffee were served Thursday, May 28, 1981, at Sacramento Shops to celebrate Machinist George Widrig's retirement after 44 years of service. George was presented with a WP jacket, a plaque from President Flannery thanking him for his loyal service to the Company, and several other gifts. The shop employees presented him with a card and \$101 to start his retirement off with a bang. We tried to take pictures of this momentous occasion, but the camera refused to

function, just couldn't believe George was really retiring. We all wish him a well deserved healthy and happy retirement.

On May 1, 1981, Al G. Slade, Car Foreman from Portola, was promoted to Assistant Shop Superintendent at Sacramento Shops. Also, congratulations to Dave Johnston on his promotion from Assistant Shop Superintendent to Mechanical Engineer, Sacramento, effective May 1, 1981.

OROVILLE - TRANSPORTATION

A. I. "Rick" Reichenbach

Our annual W.P. Picnic is passed and we received many nice remarks from out of towners, plus our locals, on the very fine outcome.

Jeff Warren, Clerk, won most all the golfing prizes and several other local golfing enthusiasts scored real well.

Many thanks to all who were instrumental in the planning and operation of this most successful Picnic.

We had a very nice addition to the world when Beth Warren, wife of Clerk Jeff Warren gave birth to Kelly, a beautiful young lady weighing in at 9½ pounds and standing 21 inches high. Born at 8:01 a.m. 4-23-81. . . Congratulations.

We recently had our annual Credit Union meeting with 75 members in attendance and with the following results:

President . . .	W. I. Bump
Vice Pres. . .	Verl Leishman
Secretary . .	M. D. Parnell
Treasurer . .	M. A. McLain
Directors . . .	J. H. Folkner
	Wm. Lutz
	C. K. Shankel
Manager . . .	Judy Quinn
Supervisory	Barbara Bye, Lois Leishman, Ruth Lightle, Clara Bump, Nellie Robinson.

Albert Tedd, retired Clerk, recently moved to Oregon for his health and we hear he is doing fine. . Keep it up, Al.

We regret to report the passing of Pedro Aguilera, retired MW employee and our condolences go to his wife, June and family.

Road Foreman Jack Belmont has been away from the premises on account of ill health but is progressing nicely according to our latest reports. . . We hope he will be back soon and we miss him. . .

Walter Hentz has been off due to illness but has returned to work and everyone wishes him well.

Grace Robinson, wife of Swman Al Robinson, won First Prize in the Horse-shoe Pitching contest at Portola Picnic. Al is all smiles.

Mike and Minnie Jayne, Retired Condr., celebrated their 58th wedding anniversary on June 6, 1981. Isn't that GREAT. . . Congratulations.

We wish to welcome Dave Henke to the Oroville Area. Dave was transferred to the Oroville Trainmaster position recently. Our former Trainmaster Jack Rich was transferred to Portola. . . Best of luck to both. . .

PORTOLA and RENO

Karen Thomas

The month of June begins the best five months of weather for Sierra Nevada residents, and what better way to start off the summer than a golf tournament and picnic. This year the annual WP Picnic, which followed the Western Pacific Golf Tournament at Greagle Meadows, was held on June 20th at the Portola City Park. WP families and friends enjoyed volleyball, swimming, tennis, shuffleboard, a playground for the kiddies, a horse-shoe contest and lively music supplied by the Union Pacific German Band.

Special thanks to the Union Pacific for making their band available and our thanks to the Reno Office personnel for transporting the band and its equipment from the Reno airport to Portola and return.

Winners of the golf tournament were: Men's low gross - G. M. Huff (1st), J. H. Rogers (2nd), S. Thomas (3rd). Men's low net - R. L. Christensen (1st), G. M. Metzendorf, Jr. (2nd), F. Reith (3rd). Men's closest to the hole - B. Isaac. Men's longest drive - T. Terhorst. Women's low gross - Martha Green (1st), Betty Treanor (2nd), Lois Quill (3rd). Women's low net - Pat Furtney (1st), Evelyn Hastings (2nd), Faye Marquis (3rd). Women's closest to the hole - Sandy Fisher. Women's longest drive - Lois Quill.

Those taking home the trophies at the horseshoe contest were: Women's Double - Pat Scoville & Mae Dillard (Champion), Janette Wilkinson & Grace Robinson (1st), Kim and Carrie Schake (2nd), Marie Lawrence & Ruth Phelps (3rd). Men's Double - Jeff Reith & Doug Weaver (Champion), Andy Kinicki & Lee Lawrence (1st), Laural & Wally Fisher (2nd), Glen Metzendorf, Jr. & Layne Shields (3rd).

Mayor Tom Hervey wrote a thank you which appeared in the Portola Reporter, complimenting on the organization of the picnic and speedy clean-up of the grounds following. Many in attendance commented on the easy access to the picnic grounds, and on the delicious food with a "German flavor", catered by George Demery.

Also taking the summer to best advantage is Conductor J. S. Ede, who began his retirement on May 23rd after 39 years of service. Mr. Ede started his career in 1942 as a Brakeman, and worked on 3rd & 5th Subs with a year in 1947 on the hi-line. Mr. & Mrs. Ede plan to continue their residence in Portola, with time for a fishing trip here and there. Best wishes on a long and well-deserved retirement!

A hearty welcome is extended to the newly appointed Trainmaster at Portola, Jack Rich, who, along with wife Gail, will soon be making their home in this area.

Congratulations to Brkm. R. R. Mlakar and wife, Debbie, upon the birth of a son, Adam Ray, born on June 19th at Eastern Plumas District Hospital in Portola. Adam weighed a healthy 7 lbs. 4½ oz.

We extend our deepest sympathy to the Ihler family upon the passing of W. R. (Bill) Ihler on May 18th. Mr. Ihler had been an active employee of Western Pacific as a Brakeman since 1955, and was a long-time resident of Portola before moving to Winnemucca.

ELKO

Theda Mueller

Brakeman Virg Bonner was running around the offices passing out cigars announcing the arrival of a new son. This is the second boy in the family and all are very excited. Mother and son are both doing great.

Ken Jewell, Communication Maintainer, wife and son recently spent their two weeks vacation in north-western Wisconsin, Ken's home state. Ken reports a wonderful time but all too short.



Engineer O. W. Perry (left) receiving congratulations on his retirement from Division Road Foreman-Engines Guy Aguire.

BLE Division 794 had a beautifully decorated cake honoring the retirement of Engineers O. W. Perry and B. F. King. Their wives, friends and coworkers joined the cake and coffee hour in the conference room listening to all the great fishing stories these two engineers were telling. Both are great fishermen and enjoy the outdoors. Fortunately both families intend making their home in Elko. Ben left town before we were able to take a picture of him, however understand they both will be featured in their engineer's magazine. Wayne Perry had 40 years on the Western Pacific and Ben had 38 years. Both men were outstanding employees and will be missed.

W. A. "Bill" Hoxsey, Conductor, Winnemucca, also decided on retirement — he too has almost 40 years of service. However, Bill recently had to have hip surgery and at the present time is still on crutches. He and his wife dropped in the office a few days ago and said he has never been so helpless as he is now but expects to be off crutches before long and feeling much better. Bill also is one of our very best employees and we will miss seeing him around. He looks forward to spending more time with his family and grandchildren.

SALT LAKE CITY

Ed Hart

First things first; we want to welcome Engineer Bill Tatomer back to work after being off for 3 months with a broken leg. The ski slopes of Utah are some of the most beautiful in the world but they are also among the most dangerous - - at least to Engineer Tatomer.

Our heartiest congratulations are extended to all of the Sale Lake graduates of 1981. We have tried not to miss any; if you are overlooked, please accept our apologies. Mark, son of Brakeman and Mrs. Karl Bunker, graduated from Hillcrest High School; Mike, son of Engineer and Mrs. Bill Coulter from Cyprus High School; Pam, daughter of Engineer and Mrs. Jay Hammond from Bountiful High School; Leslie, daughter of Eng. and Mrs. Ted Hiatt from Bingham High School; Kelli, daughter of Engr. and Mrs. Ed Hart from Cottonwood High School; and Tad, son of Brakeman and Mrs. Larry Stauffer from Hillcrest High School. Tad also has the honor of holding the state record in power weight and olympic weight lifting in the 112# class. Brakeman and Mrs. Robert L. Jones' son, Robert, Jr. graduated with a degree in Police Science from Weber State University.

How does 9 people and 13 suitcases in a station wagon appeal to you? Mark and Kay Brunner with Dawn, Joe and Julie and Gary and Cass Cannon with Roland and Lane can tell you all about it. Their long planned trip to Washington, D. C. and points east finally materialized this summer. They flew to Washington on the 29th of May where, after a tour of the Washington area, they picked up their car for a tour of the East. They traveled to New York, Boston and even got into the state of Maine. The east coast will probably never be the same again. They flew home from Boston on the 13th of June, weary but knowledgeable about revolutionary history.

A railroad career that spanned 39 years ended on April 30, 1981 at 4 A.M. when Engineer Harold O. Smith, Salt Lake Division, brought the WPE into the North Yard for the last time.

H.O. made his first trip as a Denver and Rio Grande fireman on December 26, 1942. He was promoted to engineer

in 1951 but left the D & RG to begin a new railroad career on the Western Pacific Railroad on July 14, 1953.



Left to right, Steve Worthington BLE local chairman, Engineer Harold O. Smith and Engineer Jack Powers. Harold holds a model presented in honor of his retirement.

Harold and his wife Lorraine have four children and ten grandchildren, all of whom enjoy spending time in the

beautiful Centerville, Utah home that they built themselves. Joyce, the oldest daughter lives in the Cottonwood Heights district of Salt Lake County, Harold D., their only son lives in San Diego, California; Jackie, their #2 daughter lives in Lyman, Wyoming and #3 daughter Laurie lives in Granger, Utah.

Retirement will bring for H. O. and Lorraine more time to pursue their very active lives. When they are not working on their home they might be found golfing, hunting, fishing or traveling. Harold's most ambitious hobby is trailer building where he is considered an expert even by professionals.

At the time of this writing the Smith clan is heading for the Calgary Stampede and a leisurely visit to all the beautiful and glorious surrounding countryside, including Lake Louise and Banff.

I am sure you join with us in wishing them a successful retirement.



Service Awards April - June 1981

45 YEAR

G. T. Clark
Conductor Stockton
R. H. Powell
Conductor Portola

40 YEAR

G. A. Oels
Engineer Portola
J. D. Work
Brakeman Portola
W. B. Gray
Mechanical Foreman Elko
O. W. Perry
Engineer Elko
A. W. Bowers
Carman Elko

35 YEAR

W. J. Funk
Store Helper Sacramento
W. B. Wolverton, Jr.
Mechanical Engineer San Francisco
R. Mlakar
Brakeman Portola
Y. Louie
Accounting Clerk San Francisco
L. R. Hall
Signalman Portola
K. L. Wilcox
Store Helper Sacramento
E. S. Lacey
Roadmaster Oakland
J. C. Hahn
Carman Pittsburg
J. J. Valerga
Chief Clerk San Francisco

30 YEAR

A. L. Vizna, Jr.
Carman Portola
A. S. Moranoff
Clerk San Francisco
W. J. Leavy
Diesel Foreman Oroville
J. B. Hansen
Sales Representative Memphis

25 YEAR

R. M. Edwards
Machinist Stockton
J. A. Flores
Track Foreman Wendover
J. E. Gonzales
Track Laborer Winnemucca
R. R. Rios
Clerk Elko
H. J. Miller
Engineer Oroville
H. D. McElhanev
Machinist Stockton
F. D. Silva
Asst. Foreman Oakland
L. T. Wright, Jr.
Machinist Oroville
C. M. Noriega
Track Laborer Oroville
L. C. Preston
Sales Representative Oakland
J. C. Dorithy
Clerk Sacramento
W. J. Tatomer
Engineer Salt Lake City
W. J. Harkness
Mgr-Mktg Svcs. Salt Lake City
C. W. Beadling, Jr.
Mgr-Mktg Svcs. Philadelphia
E. A. Hastings
Engineer Elko
J. Yonan
Trainmaster Modesto
D. L. Ward
Conductor Portola
R. H. Cassidy
Conductor Stockton
T. M. Cutter
Mgr-Employee Asst. Stockton
S. P. Garteiz
Conductor Portola

20 YEAR

C. L. Wade
Machine Operator Oroville
R. M. Moreno
Electrician Stockton
K. C. Baumgardner
Clerk Elko
H. L. Lightle
Section Stockman Oroville
R. C. Valencia
Supvr-Purchasing San Francisco

15 YEAR

B. M. Brown
Division Engineer Keddie
J. Dennie
Timekeeper San Francisco
R. E. Shideler
Asst. Chf Mechanical Sacramento
Officer - Cars
J. C. Riens
Brakeman Sacramento
R. G. Sorensen
Brakeman Salt Lake City
C. M. Hillyer
Brakeman Oroville
C. E. Carlock
Carpenter Sacramento

L. Hyllen
Brakeman Stockton

L. I. Seek
Machinist Stockton

D. L. Servia
Track Laborer Portola

W. S. Ivey
Clerk Stockton

S. Fernandez
Track Laborer Oakland

A. G. Ray
Brakeman Oakland

D. D. Dyer
Carman Sacramento

D. R. Edwards
Track Laborer Sacramento

A. R. Fonseca
Brakeman Stockton

C. E. Vice
Carman Elko

J. B. Robinson
Brakeman Salt Lake City

T. J. Pope
Brakeman Salt Lake City

R. F. Williams
Brakeman Milpitas

F. P. Palomino
Brakeman Fremont

J. T. Ruiz
Track Laborer Stockton

10 YEAR

C. R. Gunnell
Brakeman Oakland

W. P. Cox
Machinist Stockton

N. J. Lucas
Brakeman (SN) Sacramento

P. R. Marquis
Transportation Supvr Milpitas

J. R. Young
Ditcher Operator Sacramento

L. D. Mathis, Jr.
Engineer Stockton

S. Martinez
Machine Operator Oroville

J. M. Simon
Brakeman Oakland

J. L. Reed
Clerk Elko

A. L. McManus
Asst. Cht Car Svc. Clk. San Francisco

Appointments

R. L. Mickley
Programmer Analyst San Francisco

G. Y. Neu
Trainmaster Elko

D. L. Johnston
Mechanical Engineer Sacramento

A. G. Slade
Asst. Shop Superintendent Sacramento

J. R. McCaul, Sr.
Asst. Shop Superintendent Stockton

W. A. Hill
Trainmaster Fremont

T. S. Marshall
Data Quality Control Trainee San Francisco

M. E. Newcomb
Supervisor-Estimated Revenue Statistics San Francisco

S. Thomas, Jr.
Assistant Trainmaster San Jose/Milpitas

L. M. McDonald
Division Administrator Sacramento

D. E. Henke
Trainmaster Oroville

J. S. Rich
Trainmaster Portola

W. J. Caroni
Executive Assistant San Francisco

D. J. Behan
Applications Programmer San Francisco

G. J. Mahan
Applications Programmer San Francisco

L. S. Brilliant
Director-General Claims San Francisco

S. N. Bettadapura
Programmer Analyst San Francisco

They Have Retired

All of us at Western Pacific wish the very best for the following employees who have retired from active service.

Gerald T. Clark
Brakeman/Conductor, Stockton May 29, 1981 45 yrs.

Almeta Crowder
Clerk, San Francisco June 15, 1981 20 yrs.

John S. Ede
Brakeman/Conductor, Portola May 23, 1981 39 yrs.

Victor P. Gray
B & B Foreman, Keddie May 28, 1981 23 yrs.

William A. Hoxsey
Brakeman, Winnemucca May 3, 1981 39 yrs.

Benjamin F. King
Engineer, Elko May 31, 1981 38 yrs.

Wayne R. Orton
Carman, Stockton May 29, 1981 35 yrs.

John A. Osterdock
Engineer, Stockton May 3, 1981 44 yrs.

Charles L. Otis
Engineer, Fremont May 1, 1981 37 yrs.

Othel W. Perry
Engineer, Elko May 29, 1981 40 yrs.

Eusevio G. Razo
Chief Investigator, S.F. June 1, 1981 28 yrs.

Glenn E. Sayne
Carman, Milpitas June 5, 1981 31 yrs.

Harold O. Smith
Engineer, Salt Lake City April 30, 1981 28 yrs.

Manuel Vasquez
Signal Storekeeper, Stockton June 30, 1981 39 yrs.

George J. Widrig
Machinist, Sacramento May 29, 1981 44 yrs.

William B. Wolverton
Mechanical Engineer, S.F. May 3, 1981 35 yrs.

"Once I Built A Railroad"

Did you know that:

Virgilia, California (M.P. 270.2) was named for Miss Virgilia Bogue, daughter of Virgil G. Bogue who was Chief Engineer of Western Pacific during its construction period. Miss Bogue was also queen of the Portola Festival staged by San Francisco in 1909.

or that . . .

The name of Floka, Nevada (M.P. 479.7) was coined from the initial syllables of Florence and Katherine, daughters of Charles M. Levey, Western Pacific President 1916-1927

or that . . .

Delle, Utah (M.P. 878.4) was first named by Bennie McBride, an old prospector who started a mining shaft in the Stansbury Mountains to the northeast. Delle is at the foot of the grade descending from Low Pass into the valley of the Great Salt Lake.

In Memoriam

Mileposts wishes to record the deaths of the following active and retired Western Pacific employees and to extend condolences to their families and friends.

Joseph E. Bramse
Engineer - Oakland Sept. 30, 1980

David W. Engebretson
Signal Maintainer - Sacramento Apr. 26, 1981

John L. Geist
Chief Dispatcher - Sacramento May 15, 1981

John Harnes
Wire Chief - San Francisco Mar. 19, 1981

Alex L. Hay
Conductor - Portola Feb. 27, 1981

Morgan O. Howell
Conductor - Portola Feb. 10, 1981

William R. Ihler
Conductor - Portola May 18, 1981

Leroy Price
Track Laborer - Oakland Apr. 1981

Kirby L. Reser
Brakeman - Elko Apr. 3, 1981

Ralph F. Sarbach
Diesel Foreman - Portola May 15, 1981

Harvey E. Saxton
Engineer - Salt Lake City Apr. 3, 1981

William J. Smith
Chief Adjuster - San Francisco Apr. 26, 1981

Raymond C. Stith
Switchman - Oakland May 15, 1981

Peter J. Thill
Locomotive Engr. - Portola Jun. 27, 1981

Matthew G. Willoughby
Clerk - Milpitas Jun. 4, 1981

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WESTERN PACIFIC MILEPOSTS

Personnel Department.
T. R. Green, Director
526 Mission Street
San Francisco, CA 94105

MILEPOST 249: This view is looking west towards the Rock Creek Bridge (center rear) at Milepost 248.67. The Rock Creek Bridge is of the deck plate girder span type consisting of 3 - 60 foot, 2 - 40 foot and 1 - 20 foot spans.

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On the Cover 25 Years Ago

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Mileposts
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YUBA CITY

CA 95991

Retired Vice President and General Manager H. C. Munson is interviewed by Bob Day, "Success Story" emcee during Western Pacific's appearance on this famous Richfield Company telecast seen in 1956